

ROAD WORK NOTES

1. SEE CITY ORDINANCE FOR DESIGN CRITERIA.
2. MINIMUM 4" THICK AB UNDER SIDEWALKS
3. MINIMUM 5' WIDE SIDEWALKS
4. MINIMUM 6" THICK HIGH DIKES/AC CURBS
5. NO STEPS IN STRUCTURES. (MH, CB, VAULTS, ETC.).
6. USE BICYCLE PROOF GRATES.

APPROVED



3/8/23

DATE

DRAFT



CITY OF
PITTSBURG

NO.	DATE	REVISION

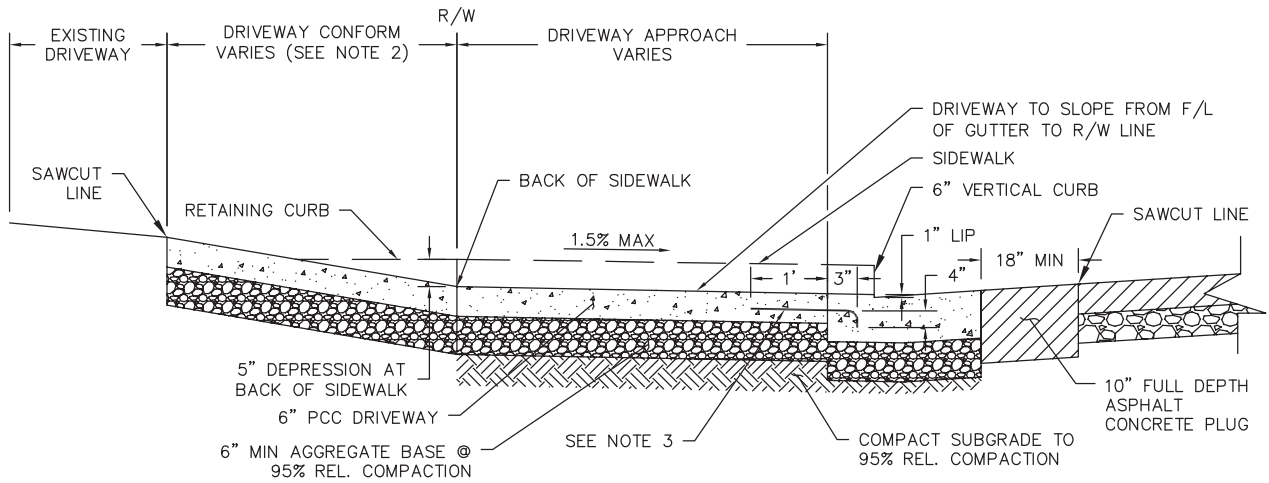
DRAWN BY:	
CHECKED BY:	MK
DATE:	2019-01-11
SCALE:	NONE

STANDARD DETAIL

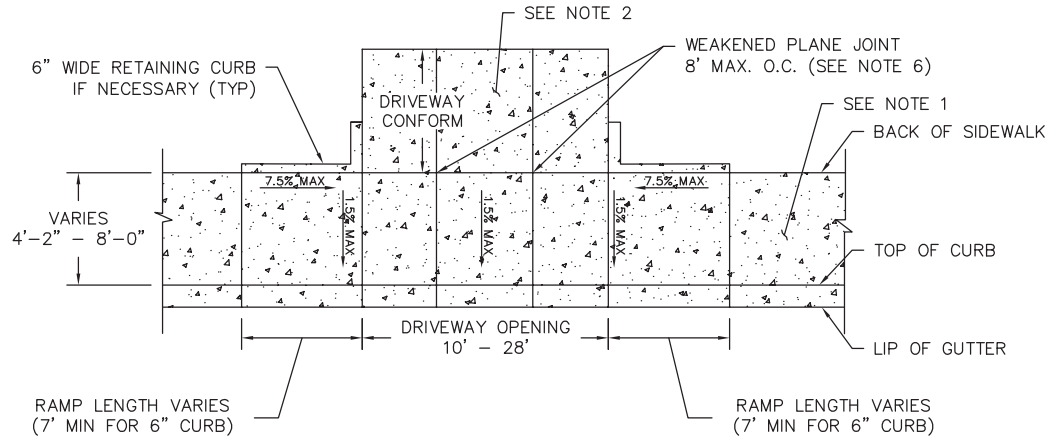
ROAD WORK NOTES

SHEET 1 OF 1

R-1



TYPICAL DRIVEWAY SECTION
SCALE: NTS



PLAN VIEW
SCALE: NTS

NOTES

1. IF DISTANCE BETWEEN RAMPS OF ADJACENT DRIVEWAYS IS LESS THAN 5', CONSTRUCT A COMMON DRIVEWAY INSTEAD.
2. CONFORM DRIVEWAY TO NEAREST WEAKENED PLANE JOINT OR SAWCUT TO SECTION OF DRIVEWAY SUCH THAT THE ALGEBRAIC DIFFERENCE BETWEEN THE FOLLOWING SLOPES DOES NOT EXCEED 10%:
 - DRIVEWAY APPROACH SLOPE AND DRIVEWAY CONFORM SLOPE
 - DRIVEWAY CONFORM SLOPE AND EXISTING DRIVEWAY SLOPE
3. #4(13) DOWELS AT 4'-0" REQUIRED IF CURB AND GUTTER ARE NOT MONOLITHICALLY POURED WITH DRIVEWAY APPROACH.
4. CONCRETE SHALL BE BROOM FINISH.
5. AGGREGATE BASE SHALL BE REQUIRED UNDER ALL RESIDENTIAL DRIVEWAYS. AGGREGATE BASE AND BASEMENT MATERIALS SHALL BE COMPACTED TO 95% RELATIVE COMPACTION.
6. SEE STANDARD DETAIL CA 70i FOR LOCATION OF WEAKENED PLANE JOINTS AND SCORE LINES.
7. WHEN REPLACING EXISTING CURB, GUTTER AND SIDEWALK, THE EXISTING CONCRETE SHALL BE REMOVED TO THE NEAREST SCORE MARK, WEAKENED PLANE OR CONSTRUCTION JOINT OR AS APPROVED BY THE ENGINEER.

APPROVED

3/8/23

DRAFT

DATE



CITY OF
PITTSBURGH

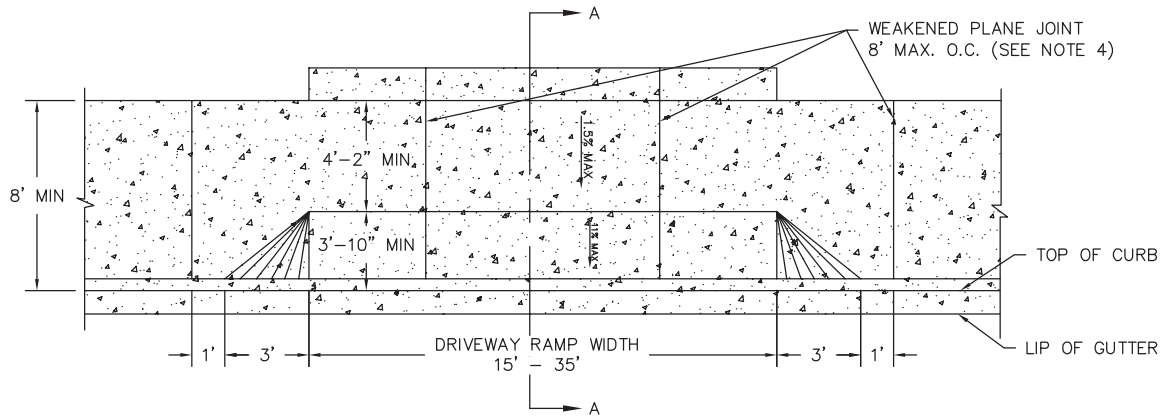
NO.	DATE	REVISION	DRAWN BY:	FY
1	2019-01-11	MAX SLOPES		
			CHECKED BY:	MK
			DATE:	2019-01-11
			SCALE:	NTS

STANDARD DETAIL

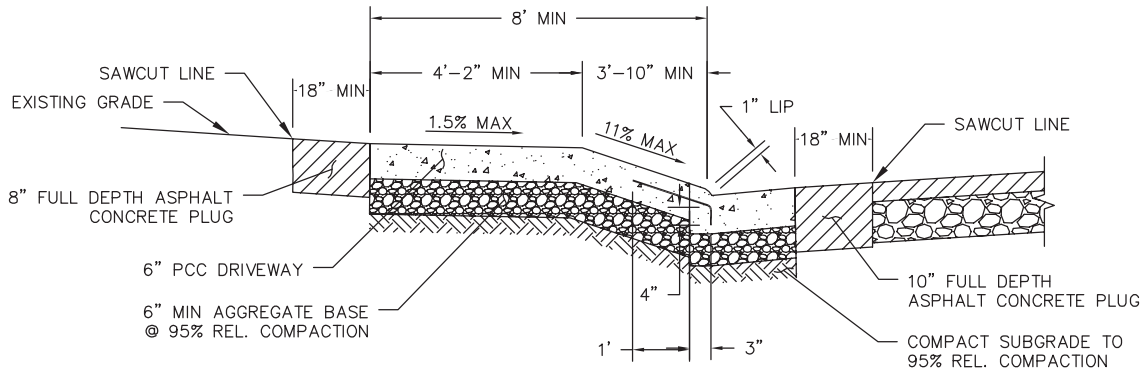
RESIDENTIAL DRIVEWAY APPROACH
- SIDEWALK WIDTH LESS THAN 8'

SHEET 1 OF 2

R-2



PLAN VIEW
SCALE: NTS



SECTION A-A
SCALE: NTS

NOTES

- #4(13) DOWELS AT 4'-0" REQUIRED IF CURB AND GUTTER ARE NOT MONOLITHICALLY POURED WITH DRIVEWAY APPROACH.
- CONCRETE SHALL BE BROOM FINISH.
- AGGREGATE BASE SHALL BE REQUIRED ON ALL RESIDENTIAL DRIVEWAYS. AGGREGATE BASE AND BASEMENT MATERIALS SHALL BE COMPACTED TO 95% RELATIVE COMPACTION.
- SEE STANDARD DETAIL CA 70i FOR LOCATION OF WEAKENED PLANE JOINTS AND SCORE LINES.
- WHEN REPLACING EXISTING CURB, GUTTER AND SIDEWALK, THE EXISTING CONCRETE SHALL BE REMOVED TO THE NEAREST SCORE MARK, WEAKENED PLANE OR CONSTRUCTION JOINT OR AS APPROVED BY THE ENGINEER.

APPROVED

3/8/23

DRAFT

DATE



CITY OF
PITTSBURGH

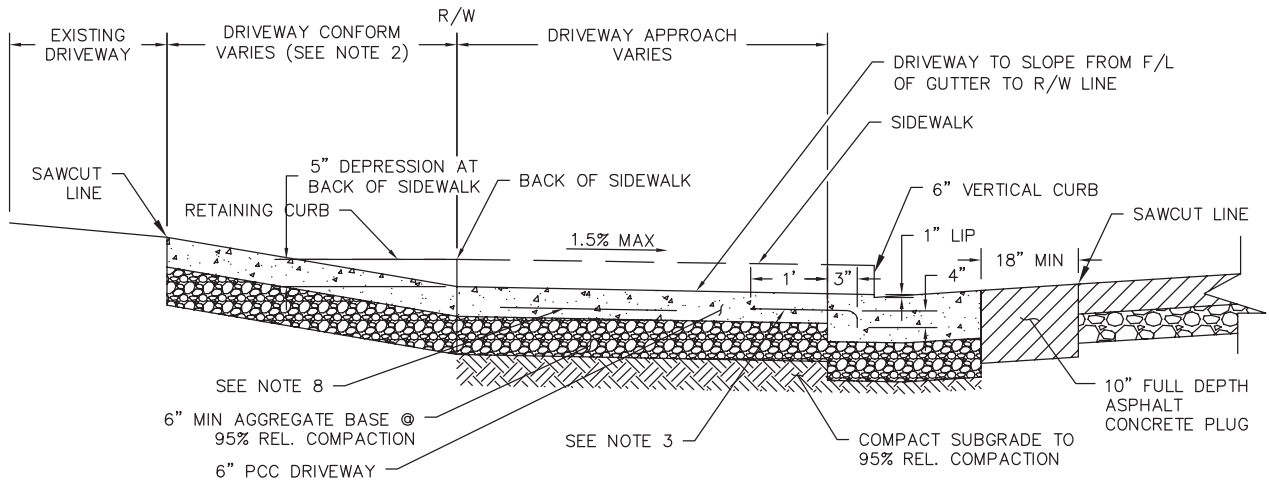
NO.	DATE	REVISION	DRAWN BY:	FY
1	2019-01-11	MAX SLOPES		
			CHECKED BY:	MK
			DATE:	2019-01-11
			SCALE:	NTS

STANDARD DETAIL

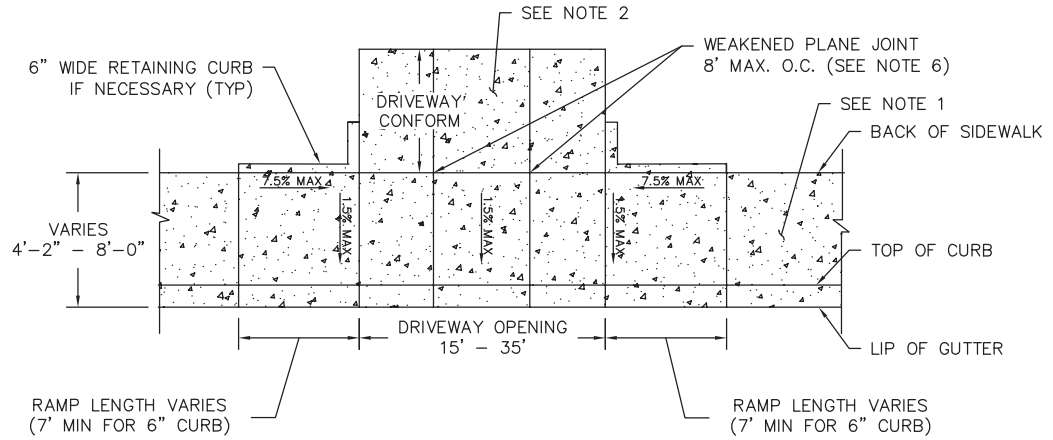
RESIDENTIAL DRIVEWAY APPROACH
- SIDEWALK WIDTH AT LEAST 8'

SHEET 2 OF 2

R-2



TYPICAL DRIVEWAY SECTION
SCALE: NTS



PLAN VIEW
SCALE: NTS

NOTES

1. IF DISTANCE BETWEEN RAMPS OF ADJACENT DRIVEWAYS IS LESS THAN 5', CONSTRUCT A COMMON DRIVEWAY INSTEAD.
2. CONFORM DRIVEWAY TO NEAREST WEAKENED PLANE JOINT OR SAWCUT TO SECTION OF DRIVEWAY SUCH THAT THE ALGEBRAIC DIFFERENCE BETWEEN THE FOLLOWING SLOPES DOES NOT EXCEED 10%:
 - DRIVEWAY APPROACH SLOPE AND DRIVEWAY CONFORM SLOPE
 - DRIVEWAY CONFORM SLOPE AND EXISTING DRIVEWAY SLOPE
3. #4(13) DOWELS AT 4'-0" REQUIRED IF CURB AND GUTTER ARE NOT MONOLITHICALLY POURED WITH DRIVEWAY APPROACH.
4. CONCRETE SHALL BE BROOM FINISH.
5. AGGREGATE BASE SHALL BE REQUIRED ON ALL RESIDENTIAL DRIVEWAYS. AGGREGATE BASE AND BASEMENT MATERIALS SHALL BE COMPACTED TO 95% RELATIVE COMPACTION.
6. SEE STANDARD DETAIL CA 70i FOR LOCATION OF WEAKENED PLANE JOINTS AND SCORE LINES.
7. WHEN REPLACING EXISTING CURB, GUTTER AND SIDEWALK, THE EXISTING CONCRETE SHALL BE REMOVED TO THE NEAREST SCORE MARK, WEAKENED PLANE OR CONSTRUCTION JOINT OR AS APPROVED BY THE ENGINEER.
8. COMMERCIAL DRIVEWAYS SHALL HAVE #4 BARS AT 18" O.C. EACH WAY.

APPROVED

3/8/23

DRAFT

DATE



CITY OF PITTSBURGH

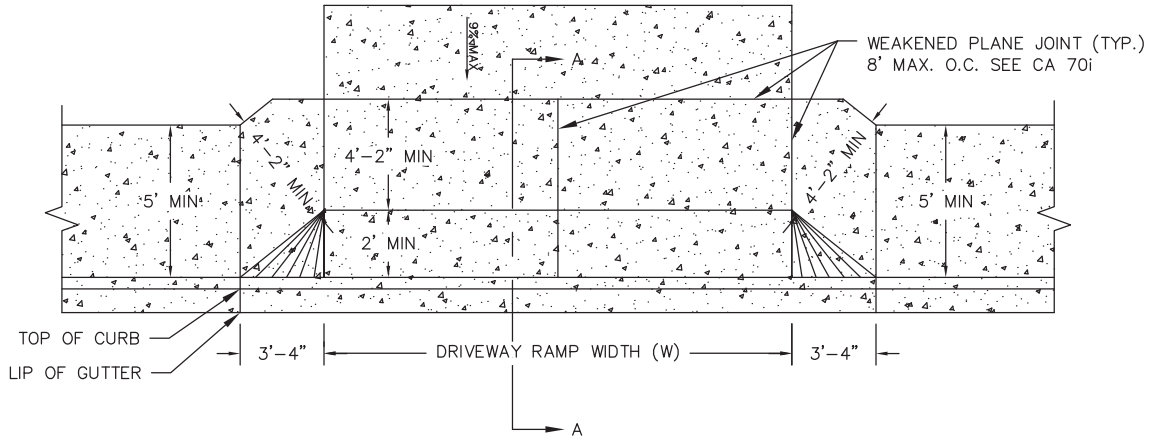
NO.	DATE	REVISION	DRAWN BY:	FY
1	2019-01-11	MAX SLOPES		
			CHECKED BY:	MK
			DATE:	2019-01-11
			SCALE:	NTS

STANDARD DETAIL

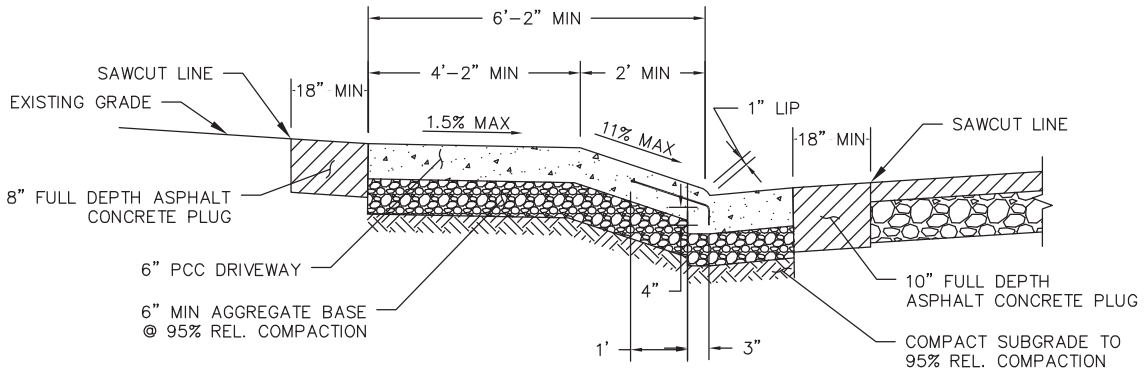
COMMERCIAL DRIVEWAY APPROACH
- SIDEWALK WIDTH LESS THAN 8'

SHEET 1 OF 2

R-3



PLAN VIEW
SCALE: NTS



SECTION A-A
SCALE: NTS

DRIVEWAY RAMP WIDTH	COMMERCIAL, INDUSTRIAL, & APO DISTRICT	COMMERCIAL, INDUSTRIAL, & APO DISTRICT
W	25' MIN 30' MAX AT 25 MPH ZONE 40' MAX AT 35 MPH ZONE	12' FOR 1-CAR GARAGE 20' FOR 2-CAR GARAGE 26' MAX ON ARTERIAL

DRAFT

APPROVED

3/8/23

DATE



CITY OF PITTSBURGH

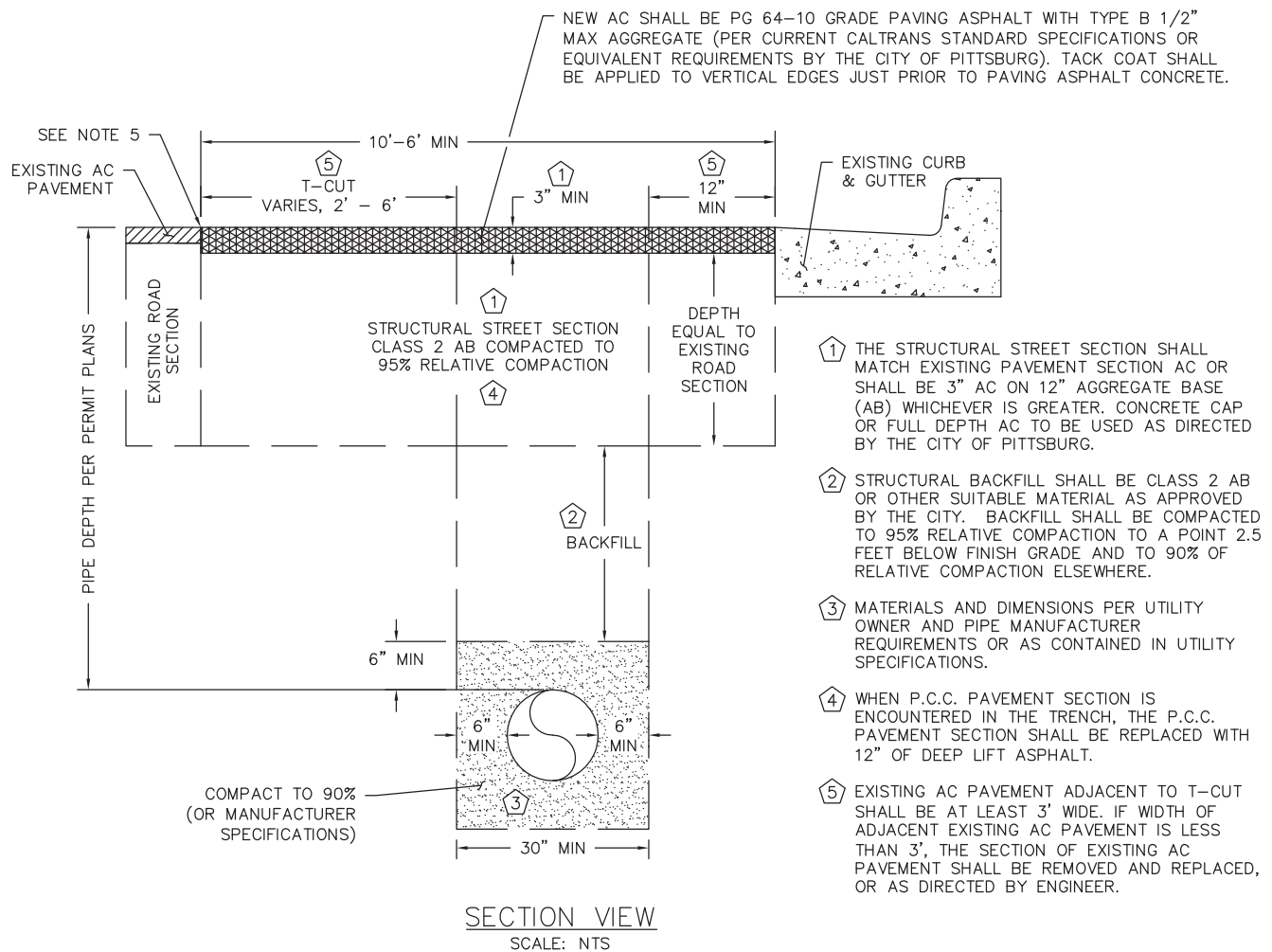
NO.	DATE	REVISION	DRAWN BY:	AW
			CHECKED BY:	MK
			DATE:	2019-01-11
			SCALE:	NTS

STANDARD DETAIL

COMMERCIAL DRIVEWAY APPROACH
- SIDEWALK WIDTH AT LEAST 8'

SHEET 2 OF 2

R-3



NOTES

1. A STREET ENCROACHMENT PERMIT IS REQUIRED FOR ALL WORK WITHIN THE PUBLIC RIGHT OF WAY.
2. ALL WORK TO BE DONE IN ACCORDANCE WITH THE CITY'S STANDARDS AND SPECIFICATIONS OR AS INDICATED IN THE STREET ENCROACHMENT PERMIT.
3. UNDERGROUND SERVICE ALERT (USA) 1-800-227-2600 SHALL BE NOTIFIED 48 HOURS PRIOR TO BEGINNING WORK.
4. ALL EXCAVATIONS SHALL CONFORM TO THE REQUIREMENTS OF CAL OSHA.
5. PRIOR TO FINISH OF PAVING, EXISTING ASPHALT SHALL BE CUT THROUGH THE FULL THICKNESS OF TO NEAT STRAIGHT LINES WITH A POWER DRIVEN SAW OR GRINDER AND REMOVED TO THE LINES OF THE T-CUT WITH THE ASPHALT REMOVED. THE EXISTING BASE MATERIAL SHALL BE COMPACTED TO 95% RELATIVE COMPACTION BEFORE PLACING PAVING. THE REPLACEMENT ASPHALT THICKNESS SHALL BE A MINIMUM OF 3" OR SHALL MATCH THE THICKNESS OF THE EXISTING PAVEMENT SECTION.
6. TRENCH BACKFILL FOR EXCAVATIONS OUTSIDE OF THE PAVED AREA MAY CONSIST OF EXISTING MATERIAL OR OTHER SUITABLE BACKFILL MATERIAL AS APPROVED BY THE INSPECTOR TO THE FINISHED GRADE. THE BEDDING MATERIAL AND DIMENSIONS SHALL BE PER THE SPECIFICATION OF THE UTILITY INSTALLING THE FACILITY. THE TRENCH BACKFILL AND BEDDING MATERIAL SHALL HAVE A MINIMUM RELATIVE COMPACTION OF 90%.
7. IF COVER IS LESS THAT 2', SPECIAL DESIGN IS REQUIRED WITH APPROVAL BY THE CITY OF PITTSBURG.

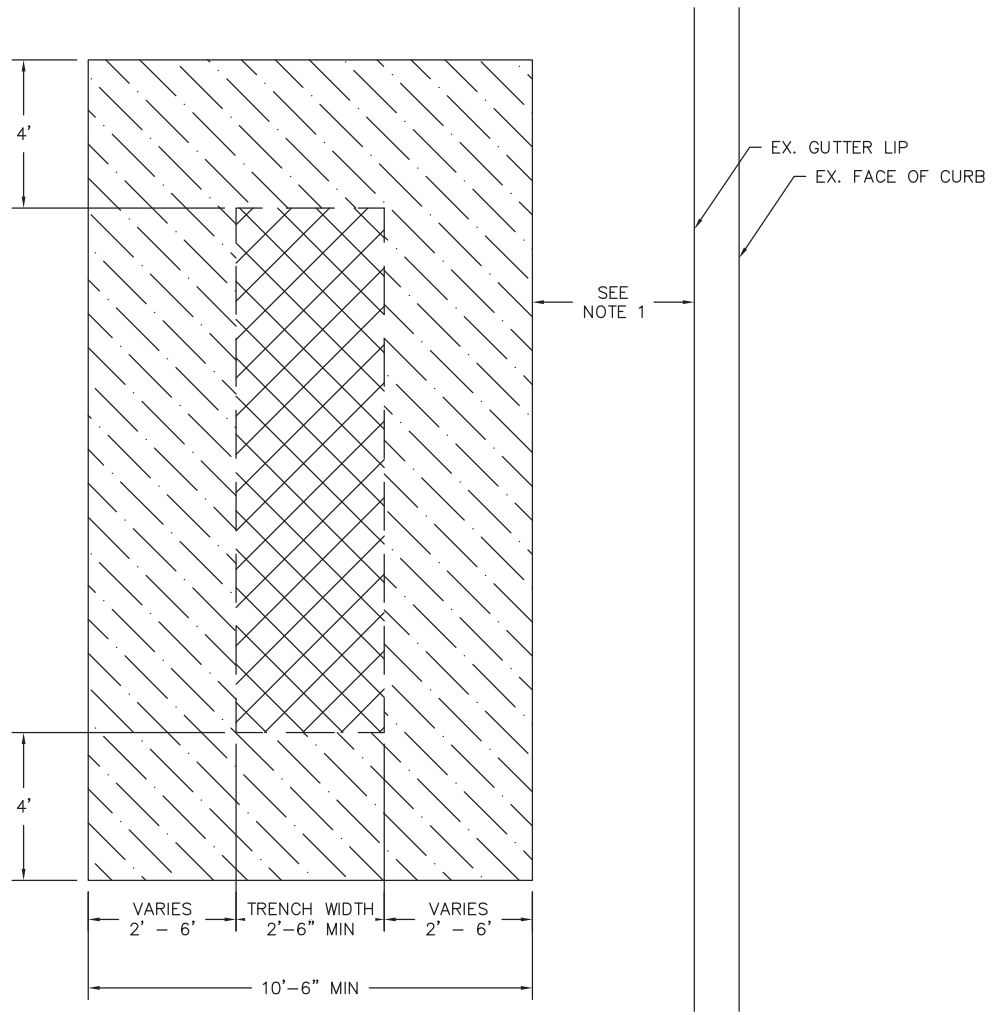
APPROVED

3/8/23

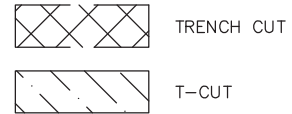
DATE

DRAFT

<p>CITY OF PITTSBURG</p>	NO.	DATE	REVISION	DRAWN BY: FY	<p>STANDARD DETAIL</p> <p>TRENCH DETAIL</p>	<p>SHEET 1 OF 8</p> <p>R-5</p>
				CHECKED BY: MK		
				DATE: 2019-01-11		
				SCALE: NTS		



PLAN VIEW
SCALE: NTS




NOTES

1. EXISTING AC PAVEMENT ADJACENT TO T-CUT SHALL BE AT LEAST 3' WIDE. IF WIDTH OF ADJACENT EXISTING AC PAVEMENT IS LESS THAN LEAST 3', THE SECTION OF EXISTING AC PAVEMENT SHALL BE REMOVED AND REPLACED, OR AS DIRECTED BY ENGINEER.

APPROVED  DATE 3/8/23

DRAFT

 CITY OF PITTSBURGH	NO.	DATE	REVISION	DRAWN BY:	FY	STANDARD DETAIL TRENCH DETAIL	SHEET 2 OF 8 R-5
	1	2008-01-10	4FT FROM TRENCH	CHECKED BY:	MK		
	2	2019-01-11	NOTE 1	DATE:	2019-01-11		
				SCALE:	NTS		

UTILITY TRENCH MASTER PERMIT CONDITIONS

SECTION 1 - Preamble

These Utility Trench Master Permit Conditions are to be used as overall, universal conditions of approval for utility trench paving repairs in the public right-of-way of the local agencies, and are part of the Encroachment Permit conditions.

SECTION 2 - Definitions

Aggregate Base (AB) - Aggregate material as defined in Sections 26 of the Caltrans Standard Specifications used for trench backfill or pavement base.

Asphalt Concrete (AC) - Pavement material that conforms to the requirements of Section 39 of the Caltrans Standard Specifications.

Base - A layer of specified material of planned thickness placed immediately below the pavement or surfacing.

Blanket Permits - Permits issued for either a specific term, and/or region, or for a group of projects, rather than a single project on a specific location, which are issued at the discretion of the local agency.

Caltrans Standard Specifications - The current version, or the version of the State of California, Department of Transportation, Standard Specifications adopted by the local agency.

Capital Projects - Special or large projects that are contracted out and thus require project specific permit conditions from the respective local agency before advertisement for bid, and would reduce the level of service to major or arterial streets during the peak hour.

Coalition - An alliance comprised of utility companies bound together by a Memorandum of Understanding (MOU) or a similar instrument and organized to negotiate with the local agencies and respond to utility trench issues as provided for in the Agreement. It is comprised of Contra Costa Central Sanitary District, East Bay Municipal Utility District, Contra Costa Water District, Pacific Gas & Electric, Comcast, SBC, and others.

Coalition Representative - The person referred to in the Master Permit Conditions who shall represent the Utilities within the Coalition and respond to the local agency regarding failure of a utility trench and its adjacent areas plus areas damaged by a utility line or facility failure.

Coordination - An affirmative effort on the part of the utilities and local agencies to coordinate, when feasible, their various projects to prevent, or minimize, repetitious trench work with emphasis on scheduling trench work on streets before the implementation of scheduled preventative maintenance pavement rehabilitation work.

Coordination Meetings - Meetings held on a quarterly basis to coordinate the activities of the utilities and the local agencies on a three (3) year planning horizon.

Encroachment Permit - A permit issued by the local agency to which these Master Conditions are attached and which contains local requirements that may relate to inspection, traffic control, insurance requirements and the like.

Latent Defect - A right-of-way pavement failure due to utility work, trenching repair or facility failure that was not apparent during the trench repair work, and following completion of work.

Life of the Trench - The useful life of the utility trench envelope is defined as the length of time the utility is in service, but no greater than the useful life of the street as defined in the Pavement Life Performance Warranty.

Local Agencies - The Cities and County of Contra Costa County.

Major Projects - Special or large projects that are either contracted out because of their size, have a length of either 300 feet or one block, or would reduce collector or arterial level of service during the peak period.

Moratorium - Following surface treatment (slurry seal, cape seal, overlay, etc.) or rehabilitation (reconstruction) of a street, the period of time when trench cuts are prohibited except as approved by the local agency as set forth in these Master Permit Conditions.


APPROVED _____



3/8/23

DATE

DRAFT

 CITY OF PITTSBURG	NO.	DATE	REVISION	DRAWN BY:	STANDARD DETAIL TRENCH DETAIL	SHEET 3 OF 8
				CHECKED BY: MK		R-5
				DATE: 2019-01-11		
				SCALE: NONE		

UTILITY TRENCH MASTER PERMIT CONDITIONS (CONT'D)

Overlay - The placement of single or multiple layers of asphalt concrete on an existing street in accordance with Section 39 of the Caltrans Standard Specifications.

Pavement Condition Index (PCI) - The numerical evaluation of the condition of the pavement based on the Metropolitan Transportation Commission model, unless specifically stipulated otherwise by a jurisdiction, wherein the pavement is rated from 0 to 100 with 70 and above being good, 55 being fair condition, 40 being poor, 25 being very poor and 10 being failed.

Pavement Life Performance Warranty - The instrument that guarantees the condition of the street along the line of the trench and the street area adjacent to as set forth in the Master Permit Conditions.

Portland Cement Concrete (PCC) - The type of pavement material described in Section 90 of the Caltrans Standard Specifications.

Reconstruct - To replace a pavement structural section.

Routine Work - Regular maintenance of facilities, emergency repairs and installation of laterals.

Structural Section - That portion of the pavement from the pavement surface to the subgrade.

Subgrade - That portion of the roadbed on which pavement surfacing, base, subbase or a layer of any other material is placed.

Total Areas Damaged - All of the areas damaged resulting from the excavation of the utility trench and the failure of the utility facility or line.

Utility Contact - The person appointed by a utility to attend and represent that utility at the coordination meetings.

SECTION 3 - Contact

3.1 General -

All surface cut work within a local agency's right-of-way requires an encroachment permit. No work shall commence until after issuance of an encroachment permit except in case of emergency, where notice by facsimile is acceptable by the next business day when work is commenced during normal non-work hours. Before commencing work, the utility shall contact and inform the local agency of the anticipated start date, particularly in the case of blanket permits. For major or capital projects, a pre-construction meeting is required unless the local agency waives this requirement. Additionally, a written notification shall be made at least five (5) working days before work starts with notice of any schedule changes at least one (1) working day prior to the original scheduled start date. For all other projects in the local agency public-right-of-way, written notification (by facsimile) shall be made at least one (1) working day in advance of the work start. Utility shall inform the local agency of the anticipated completion date.

3.2 Applicant-

The applicant for the encroachment permit, whether a contractor for, employee of, or agent of the utility, obligates the utility to these conditions.

3.3 Utility Contact-

As provided for in the Master Permit Conditions, there shall be an established Utility Contact person at each utility to ensure that the proper people are notified within a utility for quarterly utility coordination meeting notices. Such meetings are for the general coordination of work. Where there is a longstanding working relationship between an individual in a local agency and a utility counterpart, the Utility Contact person shall be kept informed of the relationship and any agreements made or understandings arrived at.

3.4 Utility Coalition Representative-

3.4.a There shall be a Utility Coalition Representative who will be a single point of contact regarding any trench failure, pavement failure adjacent to the trench and for expediting trench repairs.


APPROVED _____



3/8/23

DATE

DRAFT

 CITY OF PITTSBURGH	NO.	DATE	REVISION	DRAWN BY:	STANDARD DETAIL TRENCH DETAIL	SHEET 4 OF 8 R-5
				CHECKED BY: MK		
				DATE: 2019-01-11		
				SCALE: NONE		

UTILITY TRENCH MASTER PERMIT CONDITIONS (CONT'D)

3.4.b The local agency will make good faith effort to assess the responsible party (e.g. through permits records), and through the use of a distribution list regularly provided to the local agency by the Utility Coalition Representative, will provide that information to the distribution and Utility Coalition Representative to expedite a response.

3.4.c In the event of dense utility alignments involving multiple utilities, the possibly impacted utilities agree to mark respective utilities, in a process similar to underground service alert, to assist the Utility Coalition Representative in locating the local agency source of problem(s).

3.5 *Capital Projects* -

There will be a separate meeting for capital projects between the particular utility and the local agency to discuss local agency requirements and coordination issues. This meeting is in addition to SECTION 4, Coordination, and is desirable to define bid requirements of the local agency for the utility capital project bid documents during design, prior to contract bidding. Such planning facilitates agency/utility project coordination and negotiations, as well as complete and clear contract documents.

SECTION 4 - Coordination

The purpose of the coordination meetings is to coordinate the local agencies' future street projects with the utilities' future maintenance and capital projects. The goal is to implement the utilities' projects ahead of the City street projects to preserve the integrity of street pavements. The planning horizon for projects to be coordinated is three (3) years. New service connections may not be identifiable in concert with long term program planning. This would include main extensions to accommodate new development. Utilities shall make developers aware of the repair requirements for cutting into any pavement. Accordingly, appropriate conditions will be imposed by the local agency on the applicant during the development review process.

Quarterly coordination meetings will be held. Coordination groups will be formed by region. The hosting local agency may chair the meeting, if necessary. The Contra Costa Transportation Authority (CCTA) Regional Transportation Planning Committee boundaries will be used. Each local agency may meet with the assembled utilities on a prearranged schedule. The regions are defined as follows:

- Transplan Oakley, Pittsburg, Antioch, Brentwood, Contra Costa County
- Transpac Concord, Walnut Creek, Pleasant Hill, Martinez, Contra Costa County
- SWAT San Ramon, Orinda, Moraga, Lafayette, Danville, Contra Costa County
- WCCTAC El Cerrito, San Pablo, Richmond, Pinole, Hercules, Contra Costa County

SECTION 5 - Final Repair Timing

5.1 *Routine Work*-

Routine work shall have trench plates and temporary plywood sidewalks removed within 72 hours of placing such plate or plywood. Temporary repair shall be by either hot mix A.C. or polymer enhanced cold asphalt cement material. The utility shall be responsible for maintaining temporary repairs. Final AC or PCC shall be placed no later than 30 days from the day the pavement is cut, and within 48 hours within the central business district unless otherwise approved by the local agency.

5.2 *Major and Capital Projects*-

Major and Capital projects shall be performed with due diligence and adequate forces on a continuous basis and without delay or schedule gap to completion in accordance with a schedule that is approved by the local agency. The schedule shall include the placement of final paving. When phasing is necessary, such phasing and the related schedule are subject to review and approval by the local agency.




APPROVED _____

3/8/23

DATE

DRAFT

 <p>CITY OF PITTSBURG</p>	NO.	DATE	REVISION	DRAWN BY:	<p align="center">STANDARD DETAIL</p> <p align="center">TRENCH DETAIL</p>	<p align="center">SHEET 5 OF 8</p> <p align="center">R-5</p>
				CHECKED BY: MK		
				DATE: 2019-01-11		
				SCALE: NONE		

UTILITY TRENCH MASTER PERMIT CONDITIONS (CONT'D)

SECTION 6 - Extent of Repair/Guarantee Process

6.1 General

The following provisions are presented as an alternative to repair of paving following utility trench construction. One of two possible trench repair alternatives may be used, as shown in Exhibits A and B. The utility will determine which trench repair alternative to use as cited below.

6.2 Repair Alternative 1: Standard Trench Detail

This trench repair shall be in accordance with Exhibit A, "Standard Trench Detail". This design requires the pavement and subgrade repair to extend one (1) foot beyond the trench, as shown.

If this alternative is used, the utility shall provide the attached Exhibit C, "Pavement Life Performance Warranty." The warranty shall be for the life of the trench or until the street is reconstructed. The useful life of the street and pavement condition shall be determined. The agreed practice is that pavement with a PCI < 35, excluding trenches, is in poor condition, needs reconstruction, and no longer has useable life. Such pavement shall not be subject to the warranty. Local agency pavement rating shall have current certification in accordance with the MTC Pavement Management System requirements.

Once a street has been reconstructed or overlaid, the street shall be deemed to be renewed, and all warranty obligations to repair trench affected pavement shall be ended for all trenches completed prior to the reconstruction.

6.3 Repair Alternative 2: Trench Cut Mitigation Detail

This trench repair shall be in accordance with the attached Exhibit B, "Trench Cut Mitigation Detail." This design requires the pavement and subgrade repair to include four (4) foot wide beyond all sides of the trench, as shown.

If Alternative 2 is used the "Pavement Life Performance Warranty" shall not be required.

6.4 Other Considerations

The PCI and pavement life cycle will be considered in post construction failures. On streets with a PCI of 35 or lower, excluding trenches, the Coalition will not have post trench repair failure responsibilities beyond the line of the trench.

6.5 Pavement Failure

In the event of a trench or adjacent pavement failure, the local agency will make a good faith effort to determine who the responsible utility is for the repair. Failing that, the local agency shall notify the Coalition Representative who shall be responsible to coordinate the repair and see to its completion. Identifying the responsible utility shall be in part through the requesting of USA markings at the failure site.

It is agreed that there will be a mutual, reasonable, and good faith effort between the responsible utility and the municipality to work together to identify total areas damaged by trench excavation or the failure of the utility facility or line. Latent defects shall be repaired by the responsible utility.

Nothing in the Master Permit Conditions precludes the Coalition from pro-actively inspecting the trench and adjacent areas and implementing repairs subject to issuance of an encroachment permit. The utilities shall work amongst themselves to allocate responsibility for the repair cost.


APPROVED _____



3/8/23

DATE

DRAFT

 CITY OF PITTSBURGH	NO.	DATE	REVISION	DRAWN BY:	<u>STANDARD DETAIL</u> TRENCH DETAIL	SHEET 6 OF 8 R-5
				CHECKED BY: MK		
				DATE: 2019-01-11		
				SCALE: NONE		

UTILITY TRENCH MASTER PERMIT CONDITIONS (CONT'D)

SECTION 7 - Moratorium / Additional Repairs for Moratorium Streets.

7.1 General

There shall be a moratorium on trenching in streets for non-emergency work that have been worked on as part of the local agency annual street maintenance program. For streets that have had a surface treatment such as a slurry seal or chip seal, the moratorium on utility cuts in the street is three (3) years. For overlaid or reconstructed streets the moratorium on utility cuts is five (5) years.

Non-emergency trench cuts on streets under the moratorium may be permitted under certain conditions, but will be subject to additional requirements as provided herein and subject to local agency approval.

7.2 Repair

When a trench cut is permitted in moratorium streets, the standard detail and the following shall apply.

7.2.a Surface treated streets shall have Type II Slurry Seal as follows:

For residential roads and streets, the entire width of the road for the length of the trench plus three feet on either end shall be slurry sealed. Specific conditions will be considered that could reasonably alter the extent required in order to achieve the end result.

For wide/multi-lane roads the slurry seal application shall be for the trench length plus three (3) for:

- the affected travel lane, or
- if multiple lanes are affected, those lanes, or
- if in the center of the street, both directions to the nearest lane line or curb to curb.

7.2.b For roads that have received an overlay or have been reconstructed, the trench length plus one (1) foot beyond the trench shall be repaired and the pavement will be ground down to accept 0.15 feet of ½ inch Type A asphalt concrete for:

- the affected lane, or
- if multiple lanes are affected, those lanes, or
- if in the center of the street has been affected, then both directions to the nearest lane line or curb to curb.

If new service connection work, including main extensions is required to service new development, the developer shall be advised that part of the utility cost will be pavement restoration in accordance with these conditions as part of the conditions of development. In the development application process, the local agency will make such repair a condition of development.

7.2.c At the option of the utility, subject to agreement by the local agency, the utility may elect to pay the equivalent value of the required slurry seal to be combined with City funds for future slurry sealing of the entire street.

SECTION 8 - Amendment of Conditions

These conditions may be amended from time to time as necessary after discussion and coordination with the utility contact.

SECTION 9 - Term of Master Permit Conditions and Extension

These Master Permit Conditions will become effective on October 1, 2005 through October 1, 2010, a five year period. 120 Days before the expiration date, the local agency will so notify the Utility Coalition, at which time the Utility Coalition may review permit conditions with the Contra Costa County City-County Engineering Advisory Committee prior to the local agency extending the Master Permit Conditions for another five year period.

APPROVED  3/8/23
DATE

DRAFT


 CITY OF PITTSBURG	NO.	DATE	REVISION	DRAWN BY:	STANDARD DETAIL TRENCH DETAIL	SHEET 7 OF 8
				CHECKED BY: MK		
				DATE: 2019-01-11		
				SCALE: NONE		
						R-5

EXHIBIT C
PAVEMENT LIFE PERFORMANCE WARRANTY

The utilities will make utility cuts, as needed, in city and County Streets for installing, maintaining, and replacing utility facilities. It is the intent of the Coalition and the Local Agencies not to trench or excavate streets within a certain period following the surface treatment or overlay or reconstruction of the streets as provided for on Section 7 of the UTILITY TRENCH MASTER PERMIT CONDITIONS, which provide a moratorium due to the following impacts:

Loss of uniformity of the monolithic structural section and the associated loss in pavement structural strength per the Study;

Disruption to businesses, residents, and traffic; and

Visual degradation resulting from the "patch effect" of trenching.

Under certain conditions, the local agency may allow utility trenches in a moratorium street if utility work is necessary before the term stated in SECTION 7, and if the street will receive the repairs required in SECTION 7.

Each utility will warrant the quality of its trench and pavement repair work in any street as described below.

- 1. WARRANTY.** The utility shall repair, replace, and restore that portion of the street, including city-owned utilities such as water, sanitary sewer, and storm drainage, damaged, directly or indirectly, by a street cut made by the utility, or its agents, as closely as possible to the condition prior to the utility's street cut throughout the useful life of the street.

The useful life of the street and pavement condition shall be determined. The agreed practice is that pavement with a PCI < 35, exclusive of trenches, is in poor condition, needs reconstruction, and no longer has useable life. Such pavement shall not be subject to the warranty. Local agency pavement rating shall have current certification in accordance with the MTC Pavement Management System requirements.

Once a street has been reconstructed or overlaid, the street shall be deemed to be renewed, and all warranty obligations to repair trench affected pavement shall be ended for all trenches completed prior to the reconstruction.


- 2. ACCEPTANCE OF WORK OR SERVICE.** The acceptance of work or services, or the payment for work or services, by the local agency shall not constitute a waiver of any provision of this Warranty.
- 3. ASSIGNMENT.** Any party shall not assign this Warranty or any party substituted, without the prior written consent of all the parties.
- 4. EXECUTION.** This warranty is effective upon execution. It is the product of negotiation and all parties are equally responsible for authorship of this Warranty. Section 1654 of the California Civil Code shall not apply to the interpretation of this Warranty.
- 5. FORUM.** Any lawsuit pertaining to any matter arising under or growing out of this Warranty shall be instituted in a court of proper jurisdiction in the appropriate County of California.
- 6. MERGER AND MODIFICATION.** This Warranty sets forth the entire agreement between the parties and supercedes all other oral or written representations. This Warranty may be modified only in a writing approved by an authorized agent and signed by all parties.
- 7. NEGATION OF PARTNERSHIP.** The local agency shall not be considered a partner or joint venture with the utility or associate by the provisions of this Warranty. No employee, contractor, officer or agent of either party shall be deemed for any purpose an employee, officer or agency of the other by this Warranty.
- 8. NON-INTEREST.** No officer or employees of the local agency shall hold any interest in this Warranty (California Government Code Section 1090).
- 9. WAIVER OF DEFAULT.** The failure of any party to enforce against another a provision of this Warranty shall not constitute a waiver of that party's right to enforce such a provision at a later time, and shall not serve to vary the term of this Warranty.

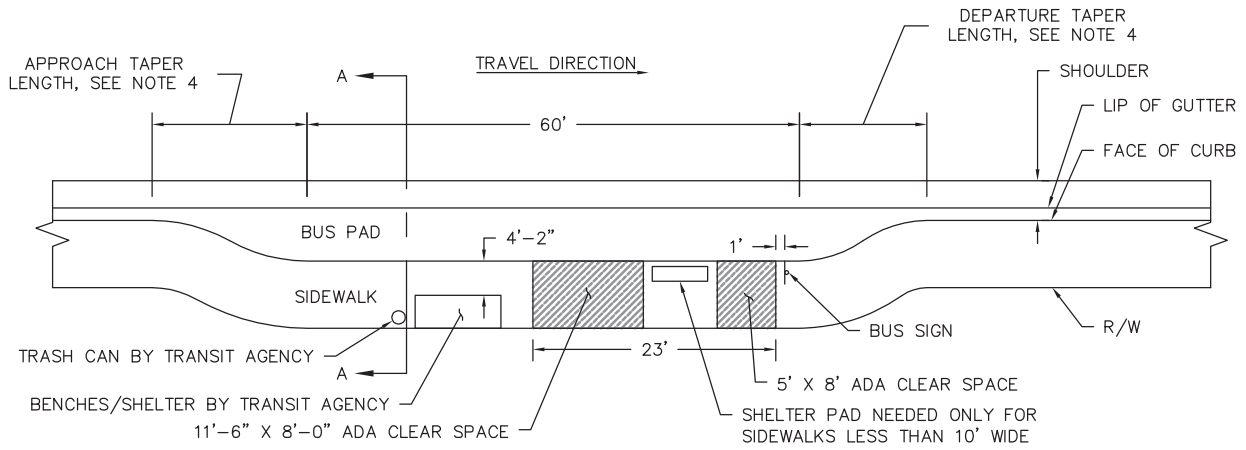
Local Agency _____ Utility _____

By: _____ By: _____

Title: _____ Title: _____

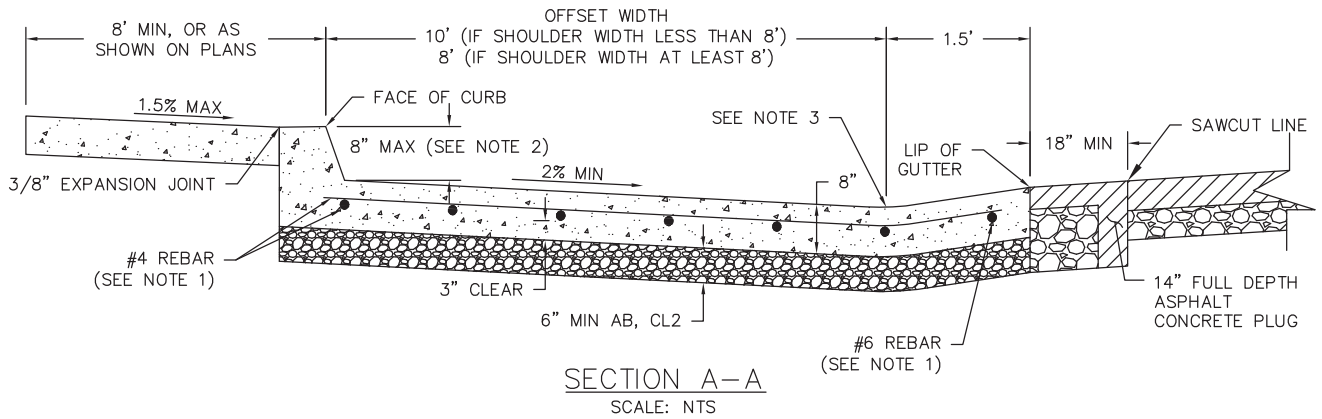
APPROVED  _____ DATE 3/8/23

DRAFT	 CITY OF PITTSBURG	NO.	DATE	REVISION	DRAWN BY:	STANDARD DETAIL TRENCH DETAIL	SHEET 8 OF 8 R-5
					CHECKED BY: MK		
					DATE: 2019-01-11		
					SCALE: NONE		



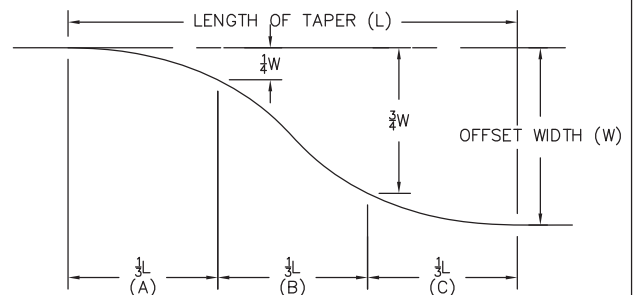
BUS TURNOUT PLAN VIEW

FOR BUSES 8'-6" WIDE AND 40' LONG



NOTES

1. CONCRETE REINFORCED WITH #4 REBAR @ 18" O.C. (BOTH WAYS) REINFORCING. ALL FREE EDGES OF BUS PAD TO BE REINFORCED WITH #6 REBAR.
2. CURB HEIGHT 8" MAXIMUM DUE TO NEEDED CLEARANCE FOR WHEELCHAIR LIFT OPERATION.
3. MAINTAIN FLOW LINE DEPTH.
4. APPROACH AND DEPARTURE TAPER LENGTHS VARY ON STREET CLASSIFICATION. SEE TABLE 1 FOR DIMENSIONS. USE PARABOLIC TAPER.
5. FOR ARTICULATED BUSES OR MULTIPLE BUSES, SEE TRI DELTA DETAILS.



STREET CLASSIFICATION	APPROACH TAPER LENGTH	DEPARTURE TAPER LENGTH
LOCAL STREET (1 - 20 MPH)	50' MIN	50' MIN
MAJOR COLLECTOR (20 - 30 MPH)	80' MIN	100' MIN
MAJOR ARTERIAL (30 - 40 MPH)	125' MIN	180' MIN

APPROVED

3/8/23

DRAFT

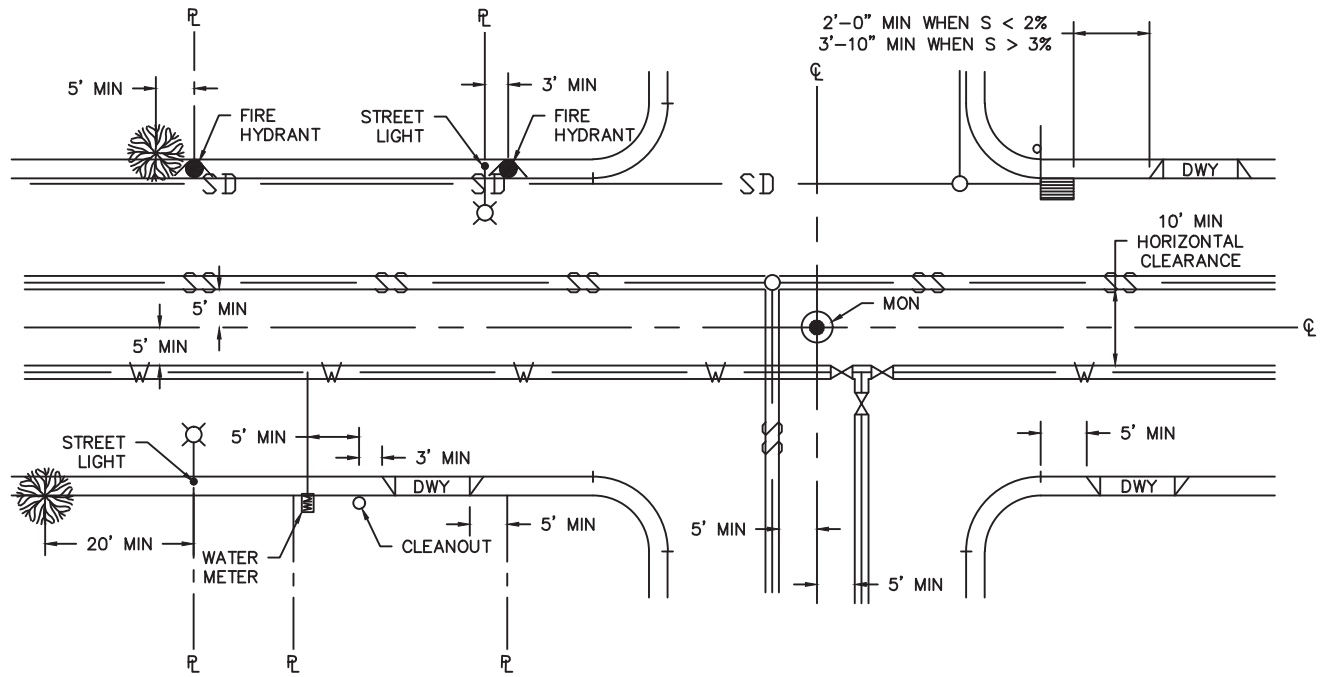
DATE

	NO.	DATE	REVISION	DRAWN BY:	JL
	1	2019-01-11	ADA CLEAR SPACE, REVISED TAPER DIMS	CHECKED BY:	MK
				DATE:	2019-01-11
				SCALE:	NTS

STANDARD DETAIL
CONCRETE BUS TURNOUT

SHEET 1 OF 1

R-6



PLAN VIEW
SCALE: NTS

NOTES

1. "TYPICAL" LOCATION OF UTILITIES ONLY.
2. SOME UTILITIES MAY NOT BE LOCATED AS SHOWN HERE.
3. DESIGNER AND CONTRACTOR TO FIELD VERIFY AND CONFIRM LOCATIONS.
4. PROPOSED DESIGNS THAT DIFFER FROM WHAT IS SHOWN HERE, NEED PRIOR APPROVAL OF CITY ENGINEER.

APPROVED

3/8/23

DATE

DRAFT



NO.	DATE	REVISION

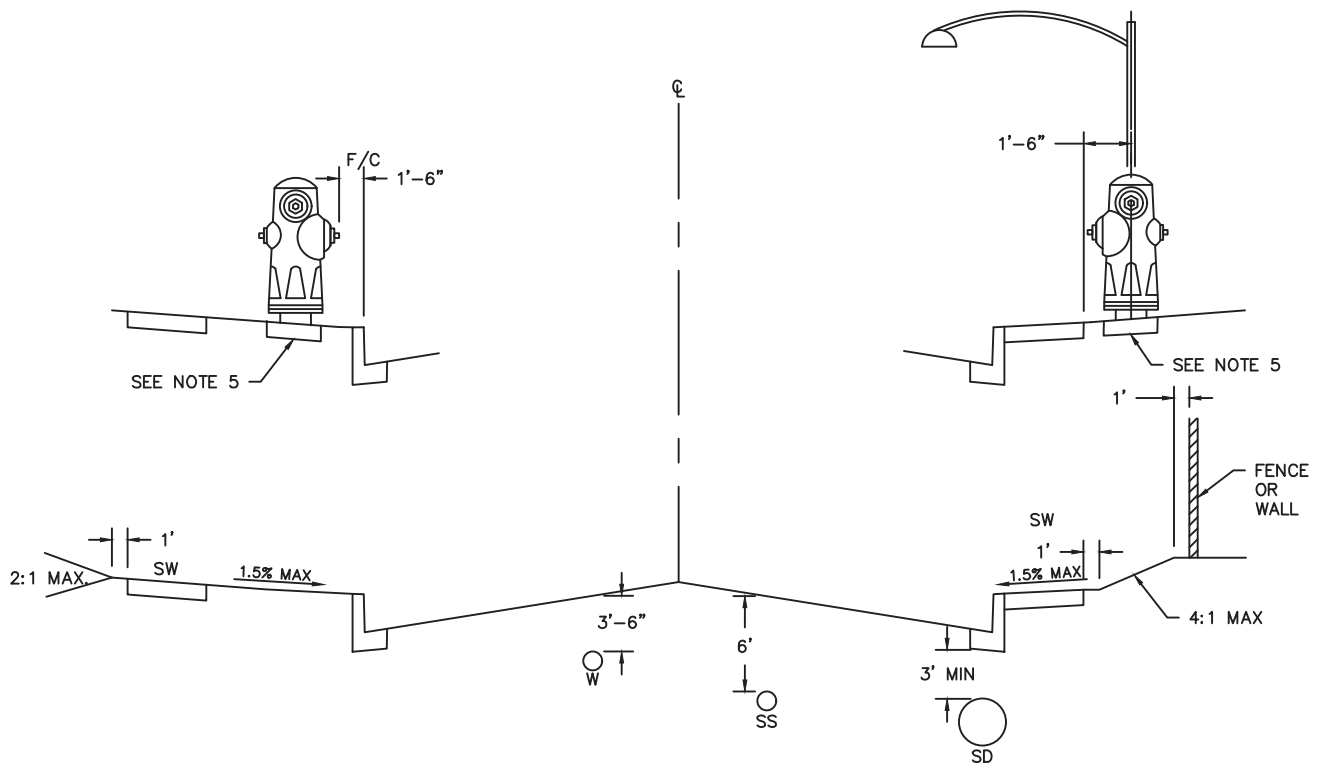
DRAWN BY:	JR
CHECKED BY:	MK
DATE:	2019-01-11
SCALE:	NTS

STANDARD DETAIL

UTILITY LAYOUT

SHEET 1 OF 2

R-7



SECTION VIEW
SCALE: NTS

NOTES

1. "TYPICAL" LOCATION OF UTILITIES ONLY.
2. SOME UTILITIES MAY NOT BE LOCATED AS SHOWN HERE.
3. DESIGNER AND CONTRACTOR TO FIELD VERIFY AND CONFIRM LOCATIONS.
4. PROPOSED DESIGNS THAT DIFFER FROM WHAT IS SHOWN HERE, NEED PRIOR APPROVAL OF CITY ENGINEER.
5. HYDRANTS SHALL BE CENTERED ON 2' SQUARE, 4" THICK CONCRETE PAD WHEN PLACED IN LANDSCAPE OR PLANTING STRIP.

APPROVED

3/8/23

DATE

DRAFT



NO.	DATE	REVISION	DRAWN BY:	AD
1	2019-01-11	CLEAR SPACES	CHECKED BY:	MK
			DATE:	2019-01-11
			SCALE:	NTS

STANDARD DETAIL

UTILITY LAYOUT

SHEET 2 OF 2

R-7

APPLICABLE CALTRANS STANDARD DETAILS

- RSP_A88A CURB RAMP DETAILS
- RSP_A88B CURB RAMP AND ISLAND PASSAGEWAY DETAILS
- RSP_A90A ACCESSIBLE PARKING OFF-STREET
- A90B ACCESSIBLE PARKING ON-STREET

APPLICABLE CONTRA COSTA COUNTY STANDARD DETAILS

- MODIFIED_CA70 STANDARD SIDEWALK DETAILS
 - CHANGE MAXIMUM CROSS SLOPES TO 1.5% (FROM 2%).
- MODIFIED_CA71 MEDIAN, CURB AND HMA DIKE DETAILS
 - CHANGE MAXIMUM CROSS SLOPES TO 1.5% (FROM 2%).
 - CHANGE DEPTH OF AGGREGATE BASE TO 6" CLASS 2 AB (FROM 3").
- MODIFIED_CA72 DRIVEWAY RAMP DETAILS
 - CHANGE MAXIMUM CROSS SLOPES TO 1.5% (FROM 2%).
 - CHANGE MAXIMUM RAMP SLOPES TO 7.5% (FROM 8.33%).
 - CHANGE DEPTH OF AGGREGATE BASE TO 6" CLASS 2 AB (FROM 3").
- CA73 CONCRETE VALLEY GUTTER DETAILS



APPROVED _____

3/8/23

DATE

DRAFT



CITY OF
PITTSBURG

NO.	DATE	REVISION	DRAWN BY:
			CHECKED BY: MK
			DATE: 2019-01-11
			SCALE: NONE

STANDARD DETAIL
APPLICABLE STANDARD DETAILS

SHEET 1 OF 1

R-8